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Logistics guideline Stadler Deutschland GmbH



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1. General

The supplier is obliged to observe and comply with the current valid Stadler Deutschland GmbH logistics guideline. In addition, the valid statutory regulations and requirements must be applied. In the event of non-compliance with the guideline and the statutory regulations, the supplier shall bear the resulting costs (e.g. processing fees, additional expenses, etc.).

1.1. Environmental Management

Stadler Deutschland GmbH is aware of its responsibility to conserve existing resources and to avoid waste, wastewater and emissions. It therefore attaches great importance to the continuous and systematic improvement of environmental protection measures, especially in the planning and optimization of load carriers and packaging solutions. Stadler Deutschland GmbH has a certified environmental management system in accordance with the currently valid DIN EN ISO 14001 and DIN EN ISO 50001 standards and ensures compliance with the principles of these standards when selecting and evaluating suppliers. Stadler Deutschland GmbH reserves the right to obtain information from suppliers regarding the life cycle assessment of its products.

1.2. Contact Partners

The respective STAP-purchaser is the first point of contact for all technical and commercial questions from the supplier. Further contacts for specific topics are named in the following chapter.

1.3. Definition of Terms

Explanations of the terms used in this document can be found in Table 1 below.

Term	Description
Stacking frame	Frame for Euro pallet
Euro pallet	Load carriers according to DIN EN 13698-1
Gitterbox pallet	Load carriers according to DIN EN 13626
KLT	Small load carrier
Loading unit	Package ready for shipment or a combination of the packing unit and the load carrier
Load carrier	Tool for internal and external transportation and storage processes
Complaint	Official letter in the event of non-compliance with requirements
Packing Unit	Created when material is packed
Packaging	Generic term for all packaging elements

Table 1: Definition of Terms

2. Loading unit, load carrier and packaging

Load units, load carriers and packaging have a protective, storage and transportation function and serve as information carriers.

Loading units, load carriers and packaging must meet the following properties and requirements before they can be used. Exceptions to this must be agreed in writing with Stadler Deutschland GmbH in advance.

2.1. General requirements for the loading units of delivery variants 1, 2 and 3¹

Loading units² must meet the following requirements:

- are to be built in such a way that qualitative and quantitative defects in the goods during transportation, storage and handling are excluded
- must withstand the dynamic and static forces during transportation and storage
- the securing of load units must comply with the legal regulations on load securing
- must be designed in such a way that the material cannot be damaged during proper transportation or storage (e.g. by impact, friction, etc.), i.e. the components must be firmly seated in the load units³
- must be able to be stacked
- securing against slipping during transport and stacking is urgently required (e.g. by strapping, shrink-wrapping or stretch-wrapping with the load carrier)
- must have a more injury-proof design (e.g. rounded corners, splinter-free)
- opening and closing of load units must be possible without tools and by one person⁴
- avoid bolting inside the load units or to the lid
- material in the load units must be clearly arranged
- loading units must have maximum volume utilization
- for delivery option 1, the weight of the individual part per package is limited to ≤ 15 kg

¹ see 2.2 Description and selection of delivery variants

² Shippable package or combination of package and load carrier

³ except for bulk goods

⁴ Safety knives are permitted

The following packaging materials can be used, among others:

- Packaging material: cardboard, foam or cardboard profiles, pads, mats, protective nets, foam or bubble wrap, intermediate layers, anti-slip mats
- Surface protection: Surface protection film, foam or cardboard profiles
- Filling material: Filling or stuffing paper from a roll, paper or bubble cushions, filling chips
- Fixing: lashing or tensioning straps, plastic strapping band
- Spacers: foam, corrugated cardboard, molded cardboard, wood

If possible, only cardboard or paper filling material should be used in cardboard packaging. If possible, Styrofoam should be avoided. Alternative packaging materials include corrugated cardboard, molded cardboard, recycled or plant-based packaging materials.

When manufacturing and using load carriers and packaging materials, care must be taken to ensure that they are designed in accordance with economic and ecological aspects. Standard load carriers should be used for this purpose.

All materials in load carriers and packaging require protection from short-term weather influences⁵ as well as from moisture, dust and dirt.

The following additional protective measures must be taken for sensitive components⁶.

Surface-treated components must always be packed individually. After approval by the purchasing department, packaging in specified call-off orders or release orders is also possible. The items must be labeled on the outer packaging (see chapter 3.1.). Sensitive electrical components must be delivered in ESD⁷ packaging. No metal clips may be used to seal the packaging.

2.1.1. Specific requirements for load carriers of delivery variant 2, 3

In general, standard load carriers should be used for all deliveries (delivery option 2), provided that the requirements in section 2.2 are met.

The basic dimensions of the load carrier must not be exceeded by packages or components (see Figure 1).

⁵ e.g. weather influences during loading and unloading

⁶ e.g. coated, painted, electronic, glazed components

⁷ Electrostatic discharge

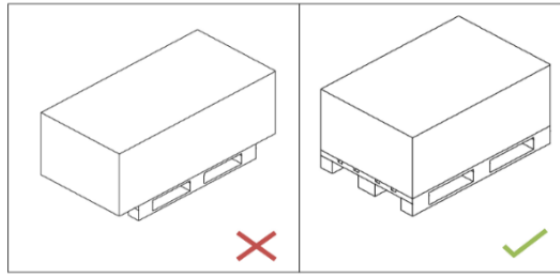


Figure 1: Basic dimensions of a load unit

Screw connections inside the load carrier or from the lid should be avoided, instead, quick-release fasteners should be used (if components are secured to load carriers using screws, ensure that these are greased when inserted).

Load carriers must be easy to handle and transportable with standard means of transportation (forklift trucks, pallet trucks, high-lift trucks) without additional securing.

Load carriers should be stackable and, if possible, collapsible. When strapping the packages or components, use plastic strapping and edge protection profiles. Furthermore, loading units of delivery variants 2 and 3 should have a stacking factor of at least 2.

Standard load carriers must be able to be picked up by the forklift truck from at least one transverse or longitudinal side (see Figure 2).

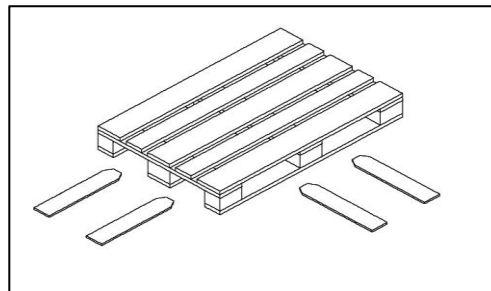


Figure 2: Picking up standard load carriers

It must be possible to pick up non-standard load carriers from at least one transverse and longitudinal side with a forklift truck, pallet truck or high-lift truck (see Figure 3).

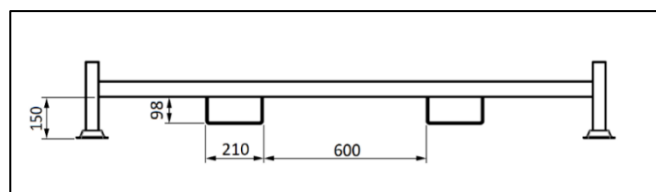


Figure 3: Non-standard load carriers

The maximum height of 1.5 meters of standard load carriers must not be exceeded (only delivery variant 2). In addition, load units of delivery variant 2 must be strapped at least once each from the long and short sides.

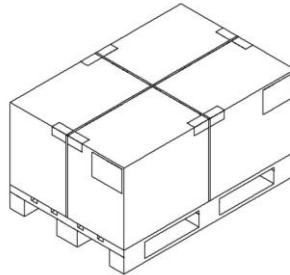


Figure 4: Load unit ready for dispatch

In addition, the load carriers must be weather-resistant to allow the empties to be stored in an unroofed environment.

With closed load carriers, the risk of mold must be taken into account (possible countermeasures: Lid with seal, cover, water drainage holes in the floor, mold-resistant surfaces).

2.1.2. Packing logic

Packaging must be filled according to type⁸ unless an alternative procedure has been agreed. Furthermore, all order items for an order must be packed individually. This applies to sensitive components. Each package must be labeled with the side from which it can be opened⁹.

An alternative procedure is used if the order items are not to be packed by type, but rather according to production, e.g. by trolley box or according to the assembly sequence; this is usually the case for materials in non-standard load carriers (delivery variant 3).

An alternative procedure is also available for set deliveries, where all parts belonging to a set (an order item consisting of several articles) must also be physically assigned to this set.

⁸ one STAP item number per package

⁹ this does not apply to transparent packaging

2.2. Description and selection of delivery variants

The delivery variants specified in Table 2 are used for orientation and to determine the correct form of delivery.

The following table 2 can be used to select a corresponding delivery variant.

Criteria for selecting the delivery variant	Delivery variant 1 (Delivery without load carrier)	Delivery variant 2 (Delivery on standard load carriers, partly with approval) ¹⁰	Delivery variant 3 (Delivery on non-standard load carriers, only with approval) ¹⁰
Approval of delivery concept ¹¹	not required	only required if module is listed under point 6.3	only required if module is listed under point 6.3
Packaging	e.g. cardboard, foil, filling material	see delivery variant 1 or separate delivery concept + approval STAP according to point 6.1	separate delivery concept + approval STAP according to point 6.1
Load carrier	no load carrier required	standard load carriers (e.g. euro pallet, mesh box pallet)	non-standard load carrier + approval STAP according to point 6.1
Loading unit Weight	≤ 15 kg and / or	≤ 350 kg and / or	approval STAP according to point 6.1
Length	≤ 600 mm x	≤ 1,200 mm x	
Width Height	≤ 600 mm x ≤ 600 mm	≤ 800 mm x ≤ 1,500 mm	
Labeling	see point 3.	see point 3.	see point 3.
Accompanying documents	see point 4.	see point 4.	see point 4.
Transportation and shipping	see point 5.	see point 5.	see point 5.

Table 2: Description of the delivery variants

¹⁰ Delivery concept must be agreed with Purchasing and Logistics

¹¹ See 2.1.1.

2.3 Creation of the delivery variants

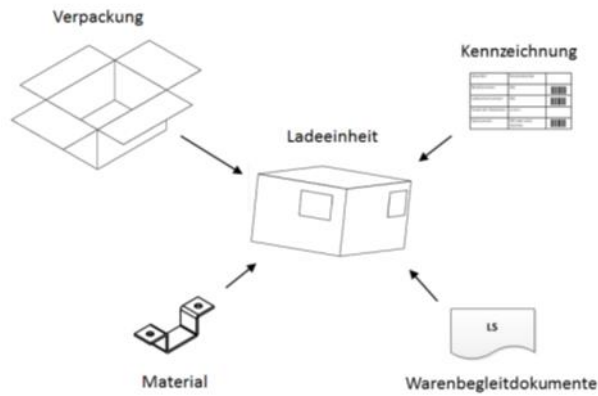


Figure 5: Creation of a load unit for delivery variant 1

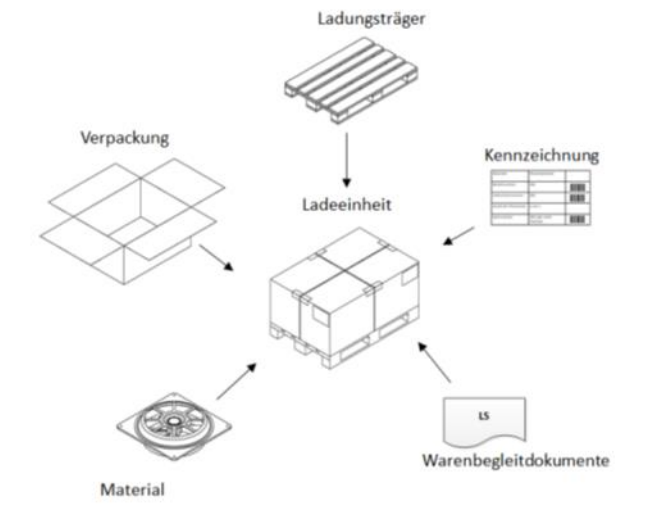


Figure 6: Creation of a load unit for delivery variant 2



Figure 7: Creation of a load unit for delivery variant 3

3. Labeling

This chapter specifies the labeling of load carriers, materials and load units. Exceptions to this must be agreed in writing with Stadler Deutschland GmbH in advance. The labeling serves the purpose of quick identification and allocation of deliveries and materials.

The labeling must be written in German or, in exceptional cases and by agreement, in English.

3.1. Labeling of the delivery parts/material

All materials must be marked with the Stadler Deutschland GmbH Product Id and, if applicable, the serial number. Labels are used for marking. The specifications are defined in this chapter.

3.1.1. Minimum requirements for Product Id and serial number labels

The requirements for the product number and serial number labels in terms of format and quality are identical. The requirements for the labels are as follows:

- Length: min. 30 mm
- Height: min. 10 mm
- Font size: min. 12 point
- Label quality: residue-free removal from the component
- Print quality: waterproof
- Barcode : Code 128 (DIN EN 799-1995)
- Spec. of barcodes waterproof, service life at least > 10 years, no non-destructive removal

The labels must be securely attached to the article or the article packaging to prevent loss. The labels must not be attached to visible surfaces or joints such as holes, glued or welded areas.

3.1.2. Marking of the delivery parts/material with the Item number

All materials must be labeled with the Stadler Deutschland GmbH Item number, identical to the order. Components that are smaller than **40 mm x 20 mm**¹² do not have to be labeled directly, but the direct packaging must be labeled:

- Text: Stadler Deutschland GmbH Item number
- Barcode: Stadler Deutschland GmbH Item number (according to the Stadler order)

3.1.3. Labeling of components requiring serial numbers

Components that are listed with a serial number due to supplier or customer specifications must also be labeled with the same number. The serial number may consist of a maximum of 18 alphanumeric characters.

The label must show the serial number and the corresponding barcode of the article. For clear identification, the serial number on the label must be preceded by the prefix "S/N:" of the serial number (e.g. S/N: 93U61456):

- Text: Prefix "S/N:" + article serial number
- Barcode: Article serial number as barcode (without prefix)

3.2. Labeling of loading units / packages

For all deliveries, each loading unit must be provided with a label in duplicate, once on the front and once on the long side (see Figure 8).

If a loading unit consists of several packages, each package must be provided with a label in duplicate (front and one on the long side) and a packing list¹³ (see Figure 9).

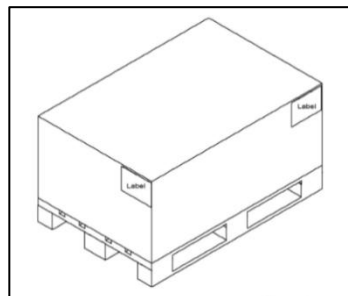


Figure 8: Labeling of load units

¹² Length x width

¹³ See 4.2 Packing list

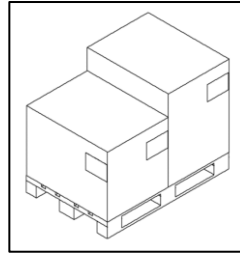


Figure 9: Labeling of packages

If the loading unit has a length of more than 1.5 m and a shifted center of gravity (starting from the center point), this must also be marked on the loading unit.

3.2.1. Minimum requirements for the label

The label must be created in the following format:

- Format: at least DIN A5 landscape format
- Paper: White
- Font size: min. 16 point
- Barcode: Code 128 or ITF

3.2.2. Content of the label

The label must contain at least the following information, in the order given:

Absender:	Musterabsender	
Bestellnummer:	001	
Lieferscheinnummer:	001	
Anzahl der Packstücke:	1 von x	
Sachnummer:	001 oder siehe Packliste	

Figure 10: Sample label

1. Sender (name)
2. STAP order number (additionally as a barcode)
3. Delivery note number (additionally as a barcode)
4. Number of packages (x/y)
5. Item number or packing list (additionally as a barcode)

3.3. Labeling of non-standard load carriers

Non-standard load carriers are only to be labeled if they are also in circulation as returnable load carriers. These load carriers must be labeled as follows:

- Type of use: Returnable load carrier
- BinMan load carrier number¹⁴
- Weight of the load carrier
- Load capacity of the load carrier (e.g. as stacking factor)
- Load on the load carrier
- External dimensions of the load carrier
- Owner of the load carrier

3.4. Other labeling

The supplier must clearly indicate special transportation and storage conditions on the package (e.g. temperature-sensitive goods). These special transport and storage conditions must be communicated to the customer in good time and in writing.

4. Shipping documents

The supplier is responsible for the proper execution of the accompanying documents. He must ensure that these are available and match the delivered goods. The accompanying documents must be written in German or, in exceptional cases and by agreement, in English.

4.1. Delivery note

If a delivery consists of several shipping units, the delivery note must always be deposited with the first unit (1/x) (including a copy). In addition, a delivery note may only contain the items of the load units that are on a vehicle (e.g. truck).

The delivery note must be created in the following format:

- Format: A4 paper
- Font: Black font
- Margin: At least a margin of 2.5 cm on the left-hand side

¹⁴ See 5.6 Empties management

The delivery note must contain at least the following information:

- Name of the sender
- Recipient (e.g. Stadler Deutschland GmbH)
- Stadler Deutschland order number
- Delivery note number
- Shipping date
- Stadler Deutschland Item number(s)
- Quantity per item number (pcs., l, kg, etc.)
- Serial numbers and barcode (if serial numbers are required)
- Best before date (if subject to best before date)
- Number of pages (x/y)
- Transportation details
- Terms of delivery
- Error message, complaint or notice of defect number

In addition, the delivery note should contain the following information, if available:

- Stadler Deutschland Drawing number(s)
- Charge code
- Stadler Deutschland Project number(s)
- Gross and net weight

4.2. Packing list

A packing list provides information on the type and scope of a loading unit. It must be enclosed with it if it contains several different materials (item numbers).

In addition to a set Item number, all associated individual part Item numbers must also be listed with quantities.

The packing list must contain at least the following information:

- Name of the sender
- Delivery note number
- Number of packages (x/y)
- Contents of the respective package¹⁵
- Quantity per Item number
- Number of position
- Serial number, if available

¹⁵ if a Stadler Deutschland GmbH Product Id is available, this must always be stated

4.3. Consignment note or waybill

The freight documents must be completed in accordance with DIN 5018:2011-04¹⁶.

4.4. Customs clearance and customs documents

The necessary customs documents must be provided for all deliveries from non-EU countries. This must be ensured by the supplier. Missing or incorrect customs documents will lead to delays or customs duties, for which the supplier is responsible and must bear the costs. Complete customs documents include the customs invoice and the proof of preference.

Contents customs invoice:

- Name and address of the sender
- Name and address of the invoice recipient
- Name and address of the recipient of the delivery
- Contact person of the sender
- EORI number (DE 4569792) and VAT ID number (DE 196736771) of the invoice recipient
- Place and date of invoicing
- Invoice number
- Customer references (order number, order number, etc.)
- Shipping method (truck, sea freight, etc.)
- Bank details and VAT number of the sender
- Terms of payment
- Incoterms® with indication of the version
- Unique article number Supplier and Stadler ID
- Customs tariff number (HS code) per article
- Meaningful product description per item
- Value of goods and currency per item with unit price information
- Country of origin and preference status per item
- Proof of preference (declaration of origin in the invoice up to €6,000 or EUR.1)
- Authorization number if approved exporter
- All additional costs, such as revision, set-up, quality, packaging, tooling, certification and analysis costs and values for materials provided
- Number of packages, volume, net and gross weight, if no packing list is created
- Information on international export controls for each item (export control number EKN, ECCN, AL number for e.g. dual-use goods)

¹⁶ Business forms - Forwarding order

Proof of preference (origin of the goods):

- Up to €6,000, a declaration must be made and signed on the customs invoice
- From € 6,000, a movement certificate EUR1 stamped by the customs office with reference to the customs invoice must be issued.
- If the supplier is an "approved exporter", the authorization number must be stated on the invoice.
- Stadler Deutschland buys duty-free.

Missing preferences can lead to customs duties and expenses for Stadler Deutschland GmbH, both of which are to be borne by the supplier.

Customs agent of Stadler Deutschland GmbH: Schenker Deutschland AG

Customs office
Hellersdorfer Weg 35
12689 Berlin
+49/30/3012995-710
zbm.berlin@dbschenker.com

5. Delivery process

5.1. Procurement process

The purchase order is the basis for all deliveries. In addition to the purchase order, the procurement process is governed by the ordering guidelines.

5.2. Outgoing goods inspection

The supplier is requested to carry out an outgoing goods inspection before dispatch and delivery to Stadler Deutschland GmbH. The "Checklist for the outgoing goods inspection of a delivery to Stadler Deutschland GmbH" listed in Attachment 3 or a comparable document can be used for this purpose. If significant deviations from this checklist are repeatedly detected, the supplier may be obliged to enclose the completed checklist with the deliveries.

5.3. Loading and unloading

When loading cargo units, the statutory regulations on load securing¹⁷ must be observed. Stacked load units must be vertical. In addition, when stacking load units, the load unit with the greatest gross weight must be at the bottom.

In addition, the load units must be loaded in such a way that unloading with a 2-ton forklift truck is possible without additional effort.

If this is not possible, this must be agreed and approved in advance with Stadler Deutschland GmbH.

5.4. Transportation and shipping

If all requirements from the previous chapters have been observed, the ready-to-ship loading units can be sent to Stadler Deutschland GmbH.

5.4.1. Inbound shipping instruction

Stadler Deutschland GmbH has concluded framework agreements with logistics service providers for the procurement of our materials. Therefore, please note the following shipping instructions for sending our EXW/FCA orders with immediate effect:

¹⁷ StVO §22

1. Goods up to 30 kg / parcel shipment / within EU / courier

Registration with TNT with our customer number: XXXXXXXXXX

Phone: 01806 900 800*

2. Goods up to 30 kg / parcel shipment /within EU / from EFTA or third country / courier

Registration with DHL with our customer number: XXXXXXXXXX

Phone: 0180 6 345300-3

Website: www.dhl.de/express

3. Goods > 30 kg / pallets / general cargo / partial consignments up to 3 loading meters or 2500 kg

Registration with DB Schenker with our customer number: XXXXXX

Phone: 033701/ 360 444

Fax: 033701/ 360 449

E-mail: grossbeeren.kundenservice@dbschenker.com

Website: <https://eschenker.dbschenker.com>

4. Partial and complete batches > 3 loading meters or 2500 kg

Registration at WWI Logistik GmbH, Lessingstr.79, D-13158 Berlin

Phone: +49 (0)3041938073

Fax. +49 (0)30/ 420 824 713

E-mail: abholauftrag@wwilog.com

Our shipment reference indicates all service provider as an **order-number and consignment-number**. We will no longer approve or adjust transport costs for the shipment of goods except the above-mentioned logistics service provider. For any inquiries please don't hesitate to contact the Stadler shipping department or the particular Stadler purchase department. Please note our current valid purchase- and order- requirements. You will find the suitable customer number for the registration at TNT, DHL or DB Schenker in our orders or, if necessary, requested from the contact person given below.

Shipping department: STAP-Versand@stadlerrail.com

5.4.2. Address/delivery address Stadler Deutschland GmbH

Stadler Deutschland GmbH
Heinz-Brandt-Strasse 6
13158 Berlin

5.4.3. Shipping of hazardous goods

When shipping dangerous goods, the current version of the ADR¹⁸ must be observed and applied. In addition, the respective legal regulations must be observed and applied.

5.5. Delivery

5.5.1. Opening hours goods receipt

The opening hours of the incoming goods department are specified with the order or can be seen in the booking tool.

5.5.2. Registration Transportation

The Contractor (or a contractor commissioned by it) undertakes to book a time slot via the booking tool provided by the Client at <https://rampman-stadler.logsol-gmbh.de>.

This is only necessary for deliveries of freight, **shipments via parcel services are not affected**. The use of the system is free of charge for you as a business partner of Stadler Deutschland GmbH. The **time slots must be booked at least 7 hours before delivery**. For bookings made later than 7 hours in advance, the client reserves the right to postpone or refuse unloading. The client generally rejects any resulting cost claims. The vehicle registered for delivery must report to the goods receiving office **at least 15 minutes** before the start of the booked time slot. If this report is made too late, the client reserves the right to postpone or refuse unloading. In the event of a refusal to unload, the Contractor or the Contractor's appointed contractor is obliged to book a new time slot within the framework of the above-mentioned specifications. The mandatory information to be provided at the time of booking shall be taken from the information letter sent to the Contractor in advance.

If no login data is available for registration and booking in the booking tool, the one-time login function can be used or the login data can be requested from the contact person listed below.

Incoming goods department : Warenannahme.STAP@stadlerrail.com

5.5.3. Goods acceptance

A prerequisite for the acceptance of the ordered deliveries is that all materials are free of qualitative and quantitative defects and that the delivery documents are complete. In addition to the requirements described in this guideline, the requirements specified in the order or in the order guideline and quality

¹⁸ European Agreement concerning the Carriage of Dangerous Goods by Road

guideline must also be met. In particular, the load carriers used must be free of damage. All incoming deliveries are only accepted subject to reservation. Defective deliveries may be returned at the supplier's expense or acceptance of the delivery may be refused. A non-defective partial delivery that is part of a defective delivery may also be returned.

5.6. Empties management

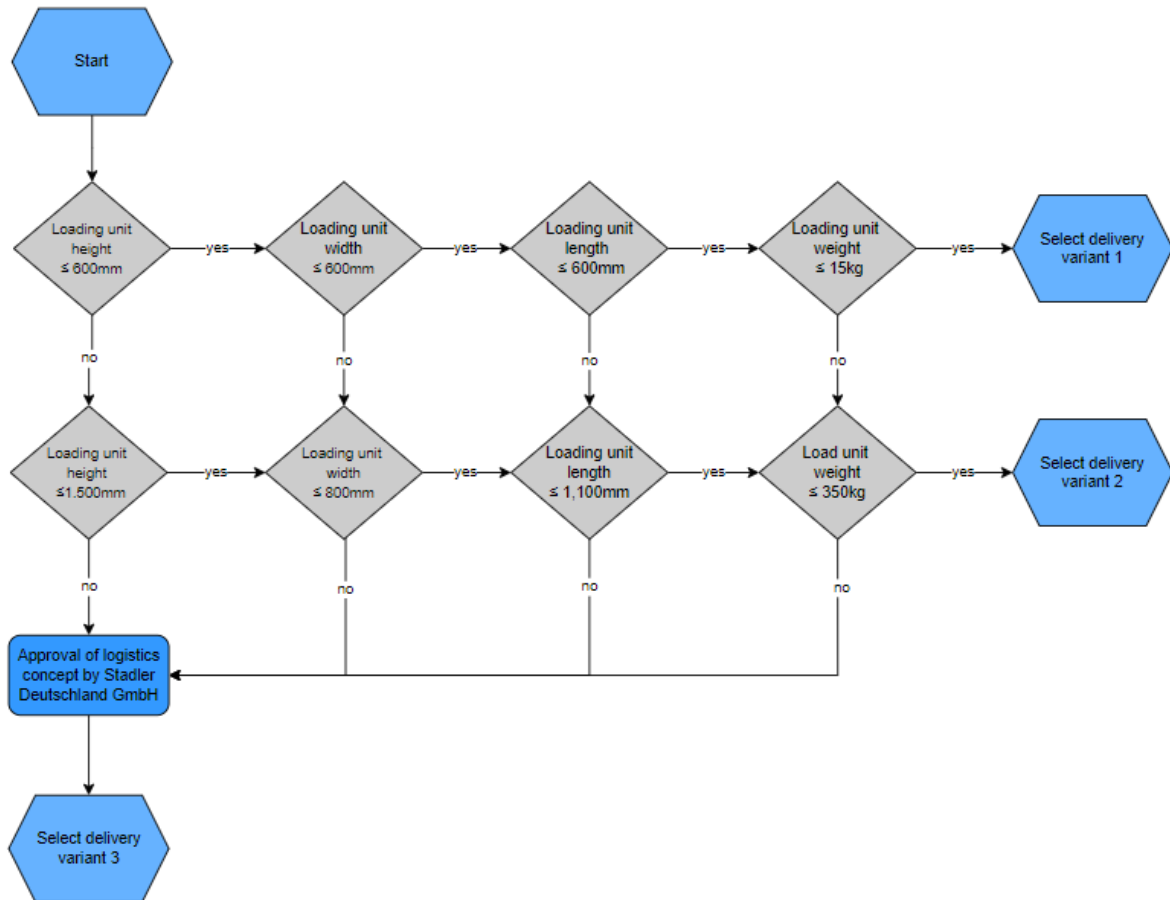
At Stadler Deutschland GmbH, the following load carriers and aids are exchangeable in the incoming goods department:

- Euro pallets according to DIN EN 13698
- Gitter box pallets according to DIN EN 13626
- Wooden stacking frame (1200 x 800 x 200) mm

If the load carriers and aids are not available in sufficient numbers for exchange, the respective supplier/forwarder will be given a signed empty voucher. This voucher entitles the holder to collect the load carriers and aids at a later date. To transmit the BinMan load carrier number and return load carriers, please get in touch with the contact person listed below.

Empties management: STAP.leergutmanagement@stadlerrail.com

6.2. Attachment 2: Selection of loading units



6.3. Attachment 3: Assemblies of delivery variants 2 and 3

The following assemblies require approval of the delivery concept by Stadler Deutschland GmbH:

Rail scraper
Battery, battery box, battery container
Floor air ducts
On-board power converter
Braking resistor
BTMS devices (Battery Thermal Management System)
Bow mask / front cabin
Cantilever
Roof panel
Ceiling, FGR ceiling system (center / side ceilings (with / without air conditioning / air duct))
Ceiling, FST ceiling system
Compressed air systems incl. carriers
E-Container
Electrical enclosures
Electrical switchboard, electrical appliances
ESS device (traction battery) roof and basement, ESS filter
Driver's desk FST, driver's desk frame, driver's desk frame, driver's desk table, driver's desk cladding
Driver's seat / instructor's seat FST
Folding passenger seats FGR
Passenger seats FGR (with / without cantilever)
Traction motor
Window panes, window rubber
Front / end coupling (center buffer coupling), short / intermediate coupling (transition coupling)
Front and side skirts
Windshield FST, windshield front light FST (in front mask / front cabin)
Floor tiles, floor covering
Luggage rack
Handrails, handrails
Heaters, fan heaters and convectors
Interior lighting
Cable duct cover
Cable ducts, cable bundles
Folding seat frame
Air conditioning duct, air ducting
Air conditioning systems, units, components
Air tank, air container, air treatment device, air supply system

Air cooling, fans, drive cooling, water cooling
Engine room scaffolding, frame, skeleton
Drive pin
Pneumatic unit
Cross member
Side boards
Seat boxes (pre-assembled)
Gap reduction, reduction, bridging, sliding and folding steps
Pantograph
Power converter (with / without integrated on-board power converter)
Support frames, scaffolding
Transformer
Partition walls
Stairs (pre-assembled)
Door operator
Door leaves, door sashes (inside / outside)
Condenser unit
Paneling, side wall or interior paneling, door drive paneling, door pillar paneling, heating paneling, engine room paneling, front wall, portal pillar, transition paneling)
Vestibule roof cover
Carriage gangway (bellows, link bridge, step plate)
Additional volume